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## Old Town Bowie Development District Standards

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### *Building Envelope Standards*

*Intent: Create an attractive development pattern compatible with the village feel and historic character of Old Town Bowie. Define the pedestrian realm with a comfortable sense of enclosure and locate buildings close to the street edge to define public space and create a pedestrian-friendly environment. Ensure that new and infill development are complementary to existing development with regard to form and siting. Allow for massing changes such as projections, recesses, storefront windows, and canopies to enhance the visual experience by providing variety of design.*

#### **A. General Design Principles and Intent**

The following statements are general intent statements meant to guide the development character of Old Town Bowie. They are meant to inform development proposals, but are not necessarily requirements for all development. Refer to the Development Thresholds in the DDOZ applicability section and the appropriate Development District Standards for the specific development standards to which the proposal(s) may be subject.

1. Buildings should be aligned and close to the street.
2. Public and private space should be clearly defined as public with open views and surveillance, or private and protected.
3. Buildings should be designed to orient views toward the street and public realm.
4. Building vehicle storage, service areas for garbage, and mechanical equipment should be located away from the street.
5. Residential parking garages should not dominate the streetscape and shall be located so as to present as little impact upon the visual environment as possible.
6. Building parking areas (off-street) should be located away from the streets and shared by multiple owners/uses whenever possible.
7. Sidewalks, curbs, and gutters should be provided on both sides of all streets whenever possible.
8. The use of alleys is encouraged whenever possible for vehicular access, for parking and loading areas, and as pedestrian paths through Old Town Bowie.

**Table 9  
Regulating Matrix**

	<b>VILLAGE STREET</b>	<b>INDUSTRIAL STREET</b>
<b>HEIGHT &amp; USE</b>	<p>Build-to Line (BTL) R-O-W</p> <p>Flat, Gable or Hip Roof Encouraged</p> <p>Residential or Office<sup>1</sup> Retail or Office<sup>2</sup></p> <p>Max. 3 Stories Min. 1 1/2</p>	<p>Build-to Line (BTL) R-O-W</p> <p>Max. 2 Stories</p> <p>Industrial/ Employment/ Commercial</p> <p>Flat, Hip, Gable, or Gambrel Roof Suggested</p>
<b>ELEMENTS &amp; FENESTRATION</b>	<p>Build-to Line (BTL) R-O-W</p> <p>Upper Stories Fenestration 40-80%</p> <p>Street Level Fenestration 70-90%</p> <p>Parking Screened by Low Wall or Greenery</p> <p>Awnings Allowed *Max 5' Projection Beyond BTL *Min 8' Clear Height Required</p>	<p>Build-to Line (BTL) R-O-W</p> <p>Front Facade Fenestration 15% Minimum</p> <p>Blank Facades Not Permitted</p>
<b>SITING</b>	<p>Build-to Line (BTL) R-O-W</p> <p>Min. 75% Building Frontage</p> <p>Street</p> <p>No Required Side or Rear Yard</p> <p>Buildable Area</p> <p>Parking on Rear of Site or in Shared Parking Areas</p> <p>Max. Lot Coverage 70%</p> <p>Lot Line</p> <p>2'-10' BTL From R-O-W</p>	<p>Build-to Line (BTL) R-O-W</p> <p>Min. 33% Building Frontage</p> <p>Street</p> <p>Parking On-Street, In Front, or To Rear</p> <p>Minimum 20' Sideyard</p> <p>Buildable Area</p> <p>Minimum 20' Sideyard</p> <p>40'-50' BTL From R-O-W</p> <p>Minimum Green Area 20% of Net Lot Area</p> <p>Lot Line</p>

\*Storefront display windows are permitted on the Village and Neighborhood streets and may project beyond the Build-To Line. This projection shall not exceed 24" in depth.

<sup>1</sup> The use categories provided above are for guidance purposes only. The table of uses for the underlying zone from the Zoning Ordinance and as modified by these Development District Standards specify the uses permitted and prohibited in Old Town Bowie. Note that the M-U-I Zone allows for a vertical mix of uses and for live/work units.

## B. Building Street Types

*Intent: The Regulating Plan (Map 13, page 188) and the Regulating Matrix (Table 9, page 74) identify the building street type for all streets within the Old Town Bowie Development District. The Building Envelope Standards regulate building height requirements, placement of uses within the building, structural elements and fenestration, and building siting. These standards recognize the unique characteristics of two distinct street types within Old Town Bowie, and establish the building form and placement upon the site for development along each street type. The Building Envelope Standards are intended to shape the pedestrian realm by creating a comfortable sense of enclosure and a pedestrian-friendly environment, with buildings placed close to the street edge and used to define public space.*

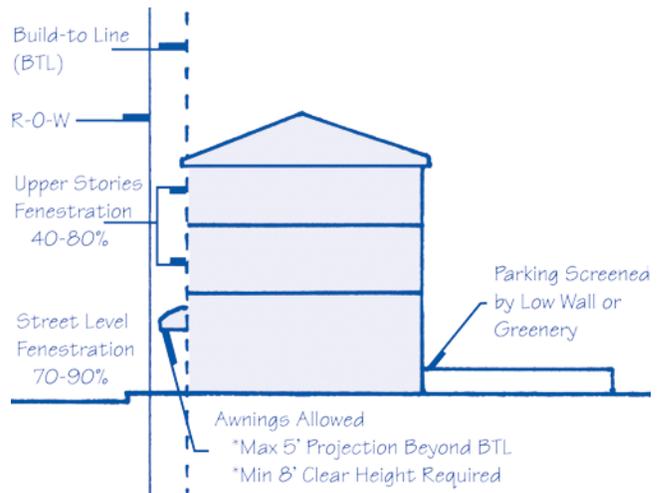
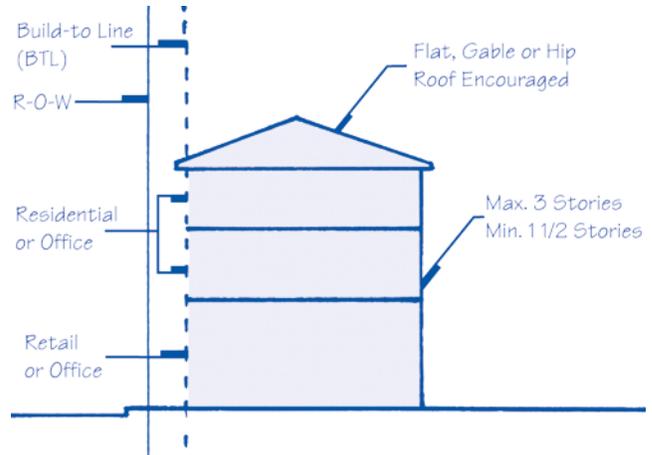
### 1. Village Street (MD 564: Lanham-Severn Road, 9th Street, Chestnut Avenue, and 11th Street)

#### a. Height and Use Specifications

- (1) Building Height: Principal building height is measured in stories. All buildings shall be a minimum of 1½ stories and a maximum of 3 stories in height.
- (2) Ground Story Height: The maximum floor-to-ceiling story height for the ground floor shall be 14 feet.
- (3) Upper Story Height: The maximum floor-to-ceiling story height shall be 12 feet for each story above the ground story.
- (4) Transition in Building Height: Where a Village Street site is within 40 feet of a single-family home, the maximum height for any structure upon the site shall not exceed 32 feet.

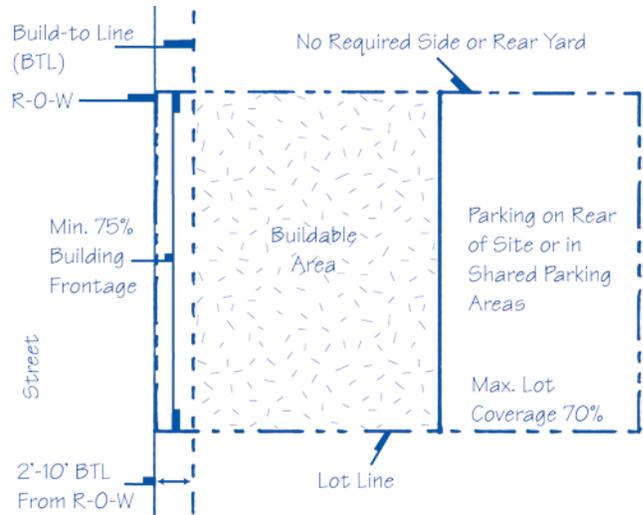
#### b. Elements and Fenestration Specifications

- (1) Ground Story Fenestration: The ground story façade shall provide between 70-90 percent fenestration for all ground floor building façades along any street frontage(s). Blank wall lengths along any street frontage(s) shall be prohibited.
- (2) Upper Story Fenestration: Upper story façades shall provide between 40-80 percent fenestration. Blank wall lengths along any street frontage(s) shall be prohibited.
- (3) Screening of Parking Areas: Parking areas located on-site shall be screened by a low masonry wall of a minimum 3-foot height, greenery such as a thick shrubbery wall, or a combination thereof.
- (4) Awnings and Canopies: Awnings and canopies may project beyond the build-to line. The maximum projection for awnings and canopies beyond the build-to line shall be five feet. Awnings and canopies shall provide a minimum clearance height of eight feet.
- (5) Storefront Display Windows: Storefront display window areas may project beyond the build-to line. This projection shall not exceed 24 inches in depth.



**c. Siting Specifications**

- (1) **Build-to Line:** A build-to line between 2 and 10 feet from the right-of-way shall be established for all buildings along the Village Street. Buildings along the Village Street shall not be located further than 10 feet from the right-of-way.
- (2) **Building Frontage:** The building street façade shall be built to a minimum of 75 percent of the overall frontage of the lot along the build-to line. The street façade shall be a single plane, limited to façade projections or recesses of less than 24 inches in depth.
- (3) **Buildable Area:** Buildings shall only occupy the building envelope delineated in the siting specifications. All buildings are required to be located along the build-to line.
- (4) **Maximum Lot Coverage:** The maximum lot coverage shall be 70 percent of the net lot area.
- (5) **Side and Rear Yards:** There shall be no minimum side or rear yard requirements and no required setbacks from the side or rear lot lines for development along the Village Street. Structures may abut the side or rear lot lines, and may share a common party wall with structures on an adjoining lot.
- (6) **Parking and Loading:** Parking shall be located on the rear of the site or in shared parking areas. On-street parking areas are encouraged if the right-of-way allows (subject to the approval of the appropriate agencies) and may also be used to satisfy any possible on-site parking requirements on a 1:1 basis. Loading areas shall be located on the rear of the site whenever possible. See the Parking Standards section for further detail.
- (7) **Front Parking:** Parking shall not be located between the sidewalk or street and the building.
- (8) **Corner lots:** Corner lots shall be treated as having street frontage on both the front and side streets and shall meet the requirements of the Village Street type on both frontages.
- (9) **Density:** In accordance with Section 27-548.23(b) of the Zoning Ordinance, the maximum residential density along the Village Street shall be limited to 12 units per acre. Sections 27-546.18(a)(4) and 27-546.18(b) of the Zoning Ordinance are replaced by this standard and do not apply within the Old Town Bowie DDOZ.
- (10) **Minimum Net Lot Area:** The minimum net lot area for multifamily residential development on property zoned M-U-I and which incorporates a vertical mix of uses (for example, residential above commercial) shall be 9,000 square feet.



## 2. Industrial Street (Zug Road)

### a. Height and Use Specifications

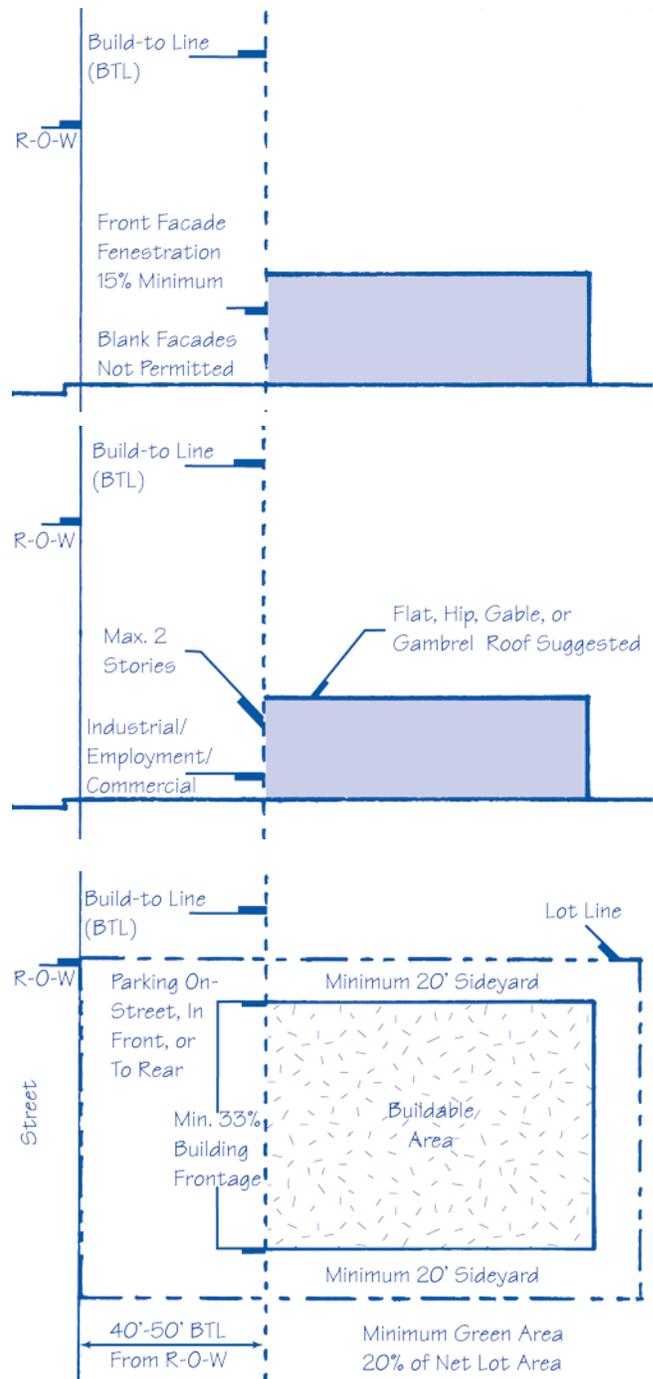
- (1) Building Height: Principal building height is measured in stories. All buildings shall be a maximum of 2 stories (or no more than 32 feet) in height, including parapets.
- (2) Ground Story Height: The maximum floor-to-ceiling story height for the ground floor shall be 20 feet.
- (3) Upper Story Height: The maximum floor-to-ceiling story height shall be 12 feet.

### b. Elements and Fenestration Specifications

- (1) Fenestration: Façades along any street frontage(s) shall provide a minimum of 15 percent fenestration. Blank façades along any street frontage(s) shall be prohibited.
- (2) Screening of Parking Areas: Parking areas located on-site shall be screened by a low masonry wall of a minimum 3-feet in height, greenery such as a thick shrubbery wall, or a combination thereof.

### c. Siting Specifications

- (1) Build-to Line: A build-to line of 40 to 50 feet from the right-of-way shall be established for all buildings along the Industrial Street.
- (2) Building Frontage: The building street façade shall be built to a minimum of 33 percent of the overall frontage of the lot along the build-to line. The street façade shall be a single plane, limited to façade projections or recesses of less than 24 inches in depth.
- (3) Buildable Area: Buildings shall only occupy the building envelope delineated in the siting specifications. All buildings are required to be located along the build-to line.
- (4) Side Yards: Side yards a minimum of 20 feet in width shall be required.
- (5) Parking and Loading: Parking shall be located on the rear of the site, in front of the structure between the sidewalk and structure, or in shared parking areas. On-street parking areas are encouraged if the right-of-way allows (subject to the approval of the appropriate agencies) and may also be used to satisfy any possible on-site parking requirements on a 1:1 basis. Loading areas shall be located on the rear of the site whenever possible. See the Parking Standards section for further detail.
- (6) Minimum Green Area: Development along the Industrial Street shall provide a minimum green area of 20 percent of the net lot area. The required green area should be contiguous whenever possible. See the Landscaping, Buffering, and Screening Standards section for further detail.



## A. General Streetscape Standards

**Intent:** Create a walkable, pedestrian-friendly village by providing a comprehensive, continuous system of sidewalks (on both sides of all streets whenever possible) and paths throughout Old Town Bowie to enhance connections and pedestrian safety. Orient buildings to the street and utilize every opportunity to create open, inviting storefronts, outdoor café seating, and interesting visual accents such as public art. Provide streetscape amenities and street furniture to encourage window-shopping and streetscape activities. Enhance safety and the visual appearance of Old Town Bowie through the provision of street trees and planting strips located between streets and sidewalks (whenever possible) to buffer pedestrians from traffic.

1. **Sidewalks:** At the time of development, the developer/property owner (including the developer and the applicant's heirs, successors, and/or assignees) is required to install sidewalks. All sidewalks along the Village Street shall be constructed using special decorative paving materials such as brick, precast pavers, Belgium block, or granite pavers. Sidewalks and paths in the rest of the Development District should not be constructed entirely of plain poured concrete and should incorporate special decorative paving materials as accent elements. Sidewalk materials should be continued across curb cuts whenever possible, and accent paving should be used to define pedestrian crossings.



2. **Streetscape Elements:** Streetscape elements (including but not limited to street trees, street furniture, landscaping and planters, decorative paving, sculpture/artwork, and bus shelters) shall be required for all development.

All proposed streetscape elements shall be indicated on detailed site plan submittals and shall include information of location, spacing, quantity, construction details, and method of illumination.

3. **Consistency of Streetscape Design Elements:** Streetscape elements shall be consistent within a development project and should be consistent throughout Old Town Bowie.

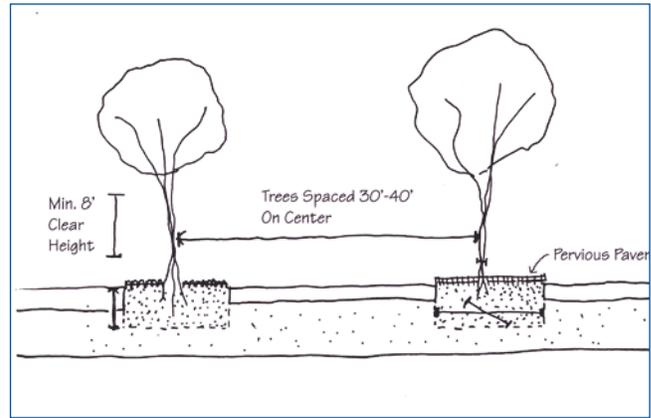


4. **Street Trees:** Street trees shall be provided along the Industrial Street to enhance and soften building façades, create street character, and to help buffer pedestrians from traffic.

Street trees shall be provided along the Village Street when the build-to line exceeds 8 feet from the edge of the right-of-way and should be provided whenever possible otherwise.

Street trees shall be planted at the time of development, shall be spaced between 30 and 40 feet on center, have a minimum caliper of 2½ to 3 inches, and a minimum clear trunk height of 8 feet. Where necessary, spacing allowances may be made to accommodate fire hydrants, utility vaults, and other infrastructure elements.

The minimum planting area for street trees shall be 5 feet in width, 8 feet in length, and 4 feet deep. Tree planting pits shall be fenced or covered with pervious pavement to protect the roots and soil and to provide additional walking surface for pedestrians when necessary. Where the plantings required above would result in an inappropriate or impractical design due to underground utilities, overhead wires, or other factors, substations may be made consistent with Standard 7 (Planting Substitutions) of the Parking and Loading Area Design section of the Parking Standards.



5. **Pedestrian-Scaled Street Lights:** Pedestrian-scaled street lighting fixtures (at heights no greater than 16 feet) shall be installed on both sides of all streets at no more than 60-foot intervals measured parallel to the street. At the time of development, the developer/property owner (including the developer and the applicant’s heirs, successors, and/or assignees) is responsible only for the installation of street lights on the side of the street that is being developed.

One consistent style of ornamental pole and luminaire should be used, to be coordinated with the appropriate public agencies.



6. **Landscape Strips:** Landscape strips are encouraged along the Village and Industrial Streets and should be at least 6 feet in width to allow for street trees and to buffer pedestrians from street traffic.

7. **Village Street Streetscape:** This plan recognizes that due to existing conditions and challenges, the desired streetscape character along the Village Street is difficult to implement on a consistent basis. Development along the Village Street shall make every effort to meet the Streetscape Standards in full.

When this is not possible, the priorities for streetscape improvements along the Village Street shall be: (1) sidewalk, (2) pedestrian-scale lighting fixtures, (3) street trees (if sufficient room is not available for the survival of street trees, seasonal displays in above-ground planter boxes should be substituted) and (4) landscape strips.



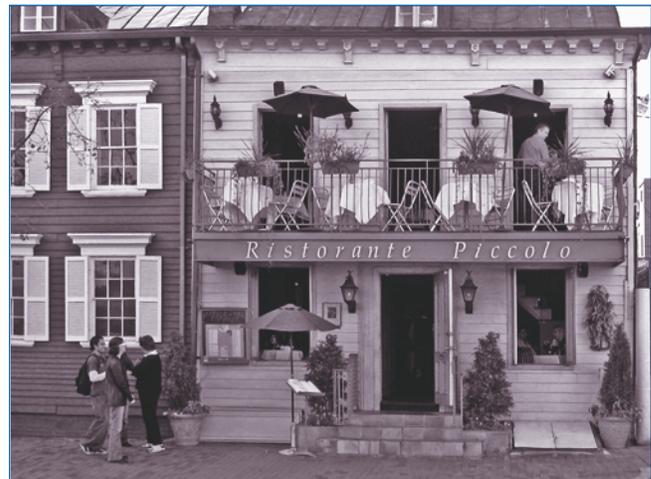
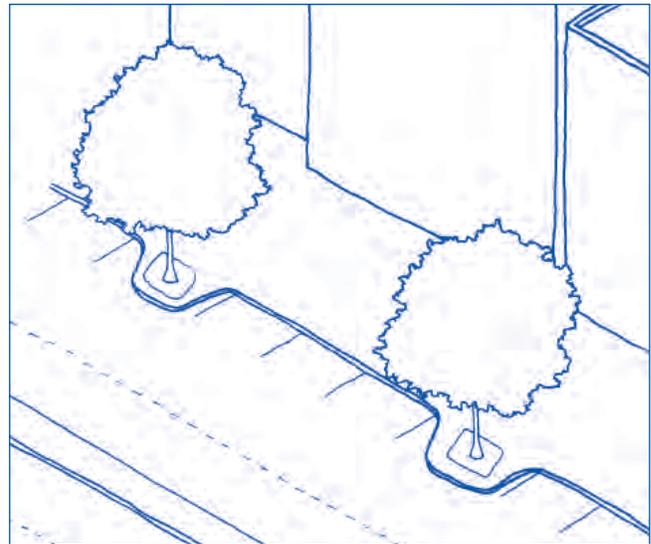
8. **Street Furniture:** All types of streetscape furniture (including but not limited to benches, bike racks, movable seating, game tables, trash receptacles, and public mailboxes) may be considered along the Village Street or in public spaces regardless of location.

Other forms of development on the Industrial Street should select from a more limited palette focused only on elements such as trash receptacles and benches placed at appropriate locations.

Developers should work with planning staff and the City of Bowie to develop this palette at the time of the first detailed site plan submittal for each of these three locations.

9. **Curb Bump-Outs:** Curb bump-outs should be incorporated into the streetscape design whenever possible to provide physical separations and mitigate the visual impact of on-street parking areas and to serve as additional tree planting areas or locations for public art and other streetscape amenities.

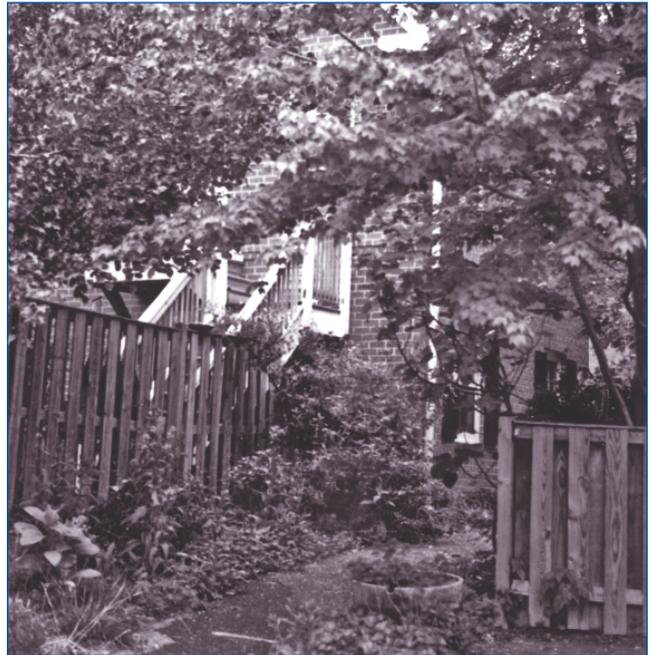
10. **Building Orientation:** The streetscape and building façade shall be the primary focus of the development. All buildings shall front the primary street(s). Buildings on corner lots shall be architecturally treated as having frontage on all façades along a street.



## B. Alleys

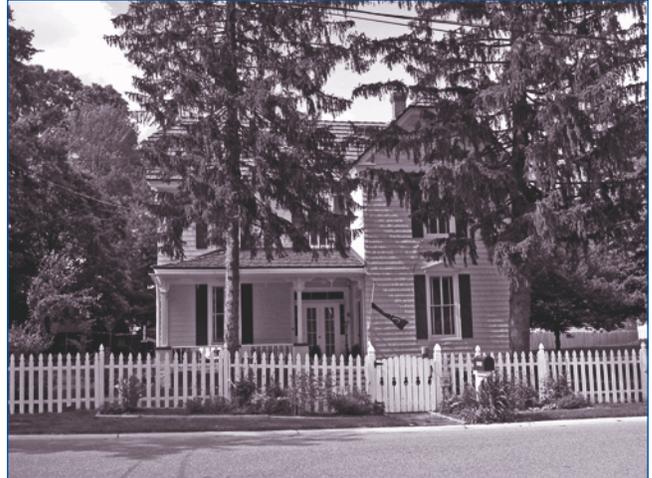
*Intent Provide safe pedestrian and bikeway access throughout Old Town Bowie by utilizing existing paper alleys as alternative pedestrian routes. Develop walkable neighborhoods with contiguous linkages that support residential sociability, commercial activity, and the use of alternative modes of transportation. Encourage the use of alleys as access routes to parking areas located behind buildings that front the streetscape.*

1. **General:** Alleys should provide access to the rear of all building lots and off-street parking facilities when possible. Alleys may be public or, if permitted by the Zoning Ordinance, private. Alley construction and right-of-way dedication shall be required of any development proposal to the extent possible.
2. **Dedicated Right-of-Way for Alleys:** Where an alley does not exist and is not constructed at the time of redevelopment of any property, the developer shall dedicate the alley right-of-way within the rear setback to the City of Bowie or to the county if the city or county will accept a public alley. If the city or county agrees to dedication, pending construction of the alley, the developer or owner shall maintain the dedicated right-of-way by, at a minimum:
  - a. Sodding and providing routine landscape maintenance to the area.
  - b. Keeping the area clear of debris, litter, stored materials, and vehicles.
3. **Alley Width:** To preserve the character of the existing pattern of pedestrian alleys within Old Town, new alleys should be no more than 18 feet in width. Pedestrian-only alleys or paths should not exceed 10 feet in width.
4. **Fences, Landscaping, and Screening:** Fences, landscaping such as hedges, and other screening methods along alleys shall be a maximum of 6 feet in height and shall be at least 20 percent visually transparent from between 4 and 6 feet in height.
5. **Screening of Service/Loading Areas From Public View:** Service and loading areas for commercial, industrial, and mixed-use buildings shall be screened from public views from alleys. Screening methods should be of quality materials.
6. **Vacated Alleys:** If a lot has expanded due to the acquisition of a vacated portion of the former alley right-of-way, the developer shall rededicate the right-of-way at the time of detailed site plan review to the City of Bowie or to the county if the city or county will accept a public alley.



7. **Crime Prevention Through Environmental Design (CPTED):** Ensure that alleys are safe for all users by designing them to comply with CPTED guidelines, including but not limited to:

- a. Providing appropriate lighting that enhances safety while minimizing light pollution.
- b. Avoiding landscaping that could create blind spots or hiding places by ensuring that trees have a minimum clear height of 8 feet and that shrubs and bushes are carefully selected and well-maintained.
- c. Accentuating visibility from adjacent homes and streets.



## Architecture and Landscape Standards

### A. General Architecture Standards

***Intent:** Guide the renovation and restoration of historic buildings and promote new façades that complement adjacent and nearby historic buildings while incorporating interesting and unique detailing and design. Provide high-quality materials and pedestrian-scaled detailing to enhance the visual appeal of development. Utilize a variety of methods such as projections and recesses, well-designed storefronts, awnings and overhangs, and appropriate building signage and lighting to enhance the pedestrian environment while ensuring new and infill development is compatible with the historic feel and scale of Old Town Bowie.*

1. **Prohibited Building Materials:** Exterior Insulation and Finish Systems (EIFS), concrete, concrete masonry units, and vinyl siding shall not be permitted.

2. **Exterior Façades:** Façades greater than 40 feet in length shall be articulated with discernible architectural elements, such as bay windows, recessed entrances and windows, display windows, arcades, balconies, plane projections and recesses, and other architectural details or articulation.

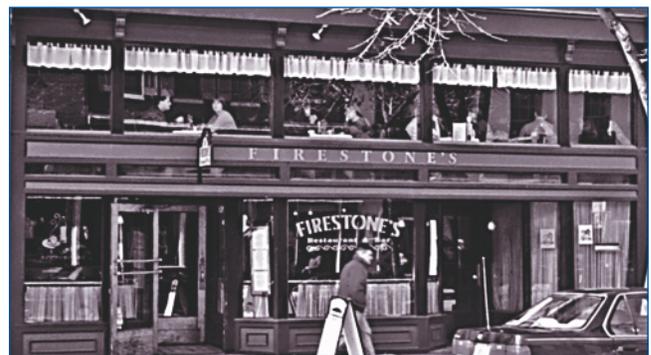
All exterior façades visible to the public (from a street, public or private open space, parking lot/structure located interior to a block, or a pedestrian alley) shall provide quality architectural materials and detailing.

Blank building walls/façades are not permitted. All façades shall include, at a minimum, at least two of the following elements: windows (including bay windows), an entry door, porch, deck, sunroom, and/or façade articulation through the use of methods such as recessed entries, pediments, and architectural detailing.

3. **Trademark Buildings:** Trademark buildings with typical franchise architecture shall conform in full to the building design standards; departures for the purpose of conforming to corporate design and architectural standards are not permitted.

4. **Windows and Entrances:** Storefronts with retail uses at street level shall provide large display windows so that people can see and be seen from the ground floor. Display windows shall encompass a minimum of 60 percent and a maximum of 90 percent of a storefront's frontage (measured in linear feet).

The main entrance shall be articulated by utilizing design elements such as transom windows, recessed entries, lighting features, architectural detailing, signs, awnings, and canopies.



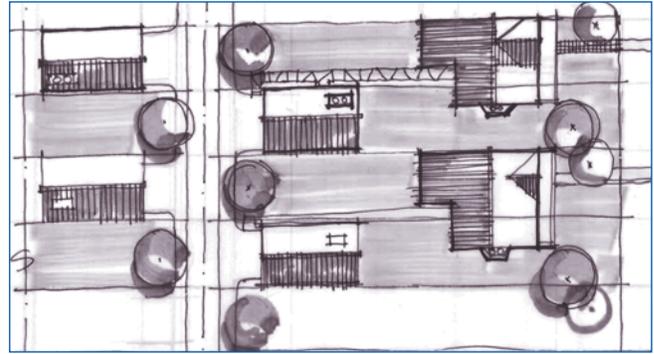
- 5. **Residential Garages:** Garages for single-family dwellings shall be located to the rear of the site.
- 6. **Building Security:** Exterior burglar bars, roll-down, accordion or sliding grates, grilles, bars, and shutters on windows and doors shall not be permitted. Other, less visually obtrusive security methods should be employed.
- 7. **Mechanical Equipment:** Mechanical equipment (such as but not limited to air compressors, pumps, transformers, meters, boxes, and HVAC units) shall be visually screened from public streets, parks, and plazas.

Screening methods may include locating equipment upon a roof behind a parapet wall or to the rear of the building, fencing, or appropriate landscaping.

Mechanical equipment shall not block pedestrian and bicyclist movement.

- 8. **Hours of Operation:** Hours of operation for industrial use within the Old Town Bowie Development District shall minimize adverse impacts on adjacent properties and the surrounding neighborhood.

Under no circumstances shall hours of operation of an industrial use begin before 6 a.m. or extend beyond 8 p.m.



## B. Signage

***Intent:** Site and building signs should contribute to a positive image for redevelopment by complementing the historical character of Old Town Bowie and the architectural composition and design of both the building and the surrounding environment. Durable, attractive, and well-maintained signs attract potential customers, provide directional orientation, and contribute to the look and feel of the community. A comprehensive wayfinding and signage system should be established for Old Town Bowie to ensure compatibility, engage pedestrian interest, encourage visitation, and enhance the experience by making it easier for visitors to find their way and get from one part of Old Town Bowie to another.*



1. **Sign Purpose:** Signs shall primarily serve to identify the name and/or type of business establishment, and building address. Signs shall be constructed of durable, high-quality materials such as brick, cut stone, stainless steel, or other similar materials.

Unless modified below, the regulations of Part 12 (Signs) of the Zoning Ordinance shall apply within the Old Town Bowie DDOZ.

2. **Signage Restrictions:** Exterior neon signs, internally lit box-style signs, and signs with moving parts, blinking lights, and animation (including LED animation) shall not be permitted.

Sections 27-613(a) and 27-613(b) of Part 12, Signs, of the Zoning Ordinance are modified as follows: Freestanding, pole-mounted commercial signs shall not be allowed, and signs located above or projecting from the roofline or parapet wall shall not be permitted.

Sign area shall not exceed the regulations of Section 27-613(c) of the Zoning Ordinance.

3. **Sign Plans:** Common sign plans shall be provided for all new institutional, office, mixed-use and retail/commercial buildings.

These common sign plans shall be accompanied by plans, sketches, or photographs indicating the design (such as colors and lettering style), size, methods of sign attachment, lighting, quantity, location on the building, and other information the Permit Review section or Planning Board (for permit review and detailed site plan review respectively) requires.

4. **Business Signage:** All businesses shall have front and/or rear entry signage such as blade and bracket, pin letter, or flat-mounted boards securely fastened to the building and oriented toward pedestrians.

5. **Window Signs:** Window signs (including letters and logos painted on storefront windows) shall not obscure the interior view of a business or retail establishment and shall not occupy more than 25 percent of the total area of the window in which the sign is located.

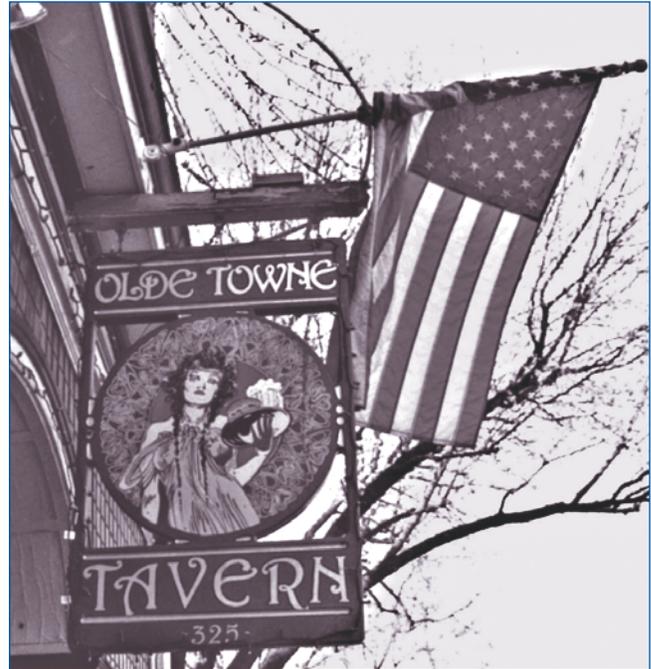
6. **Sign Lighting:** Signs shall be lit externally, so that the light does not exceed the area of the sign or spill onto the building façade. External lighting fixtures used to illuminate signage shall provide full cut-off fixtures to reduce sky glow and glare.

7. **Projecting Signs:** Section 27-613(d) of the Zoning Ordinance is modified as follows:

Signs on buildings shall project no more than 40 inches from the vertical plane of the wall to which they are attached.

Projecting signs shall not be attached to canopies; rather, canopy and awning signs, which may contain the name of a business and logo, may be located on the front face of an awning. Signs on awnings shall not exceed the maximum projection for awnings and canopies beyond the build-to line (5 feet; see Building Envelope and Block Standards).

Projecting signs shall extend not closer than 5 feet to the vertical plane of the street line and shall have a minimum clearance of 8 feet above the finished grade of a sidewalk.



## C. Lighting

**Intent:** Buildings and communities should incorporate distinctive lighting as a cohesive element of their architectural and environmental design to strengthen the appearance and functionality of the structure and its surroundings while providing adequate safety and visibility. Lighting should be designed to create a soft, evenly lit environment to ensure adequate safety and visibility along sidewalks, alleys and paths, and around building entrances and façades. Light fixtures should be constructed of attractive, high-quality materials, be incorporated into the overall design of the project, direct glare away from adjoining properties and public rights-of-way, and reduce light pollution. One consistent style or palette of lighting fixtures should be used, to be coordinated with the appropriate public agencies.

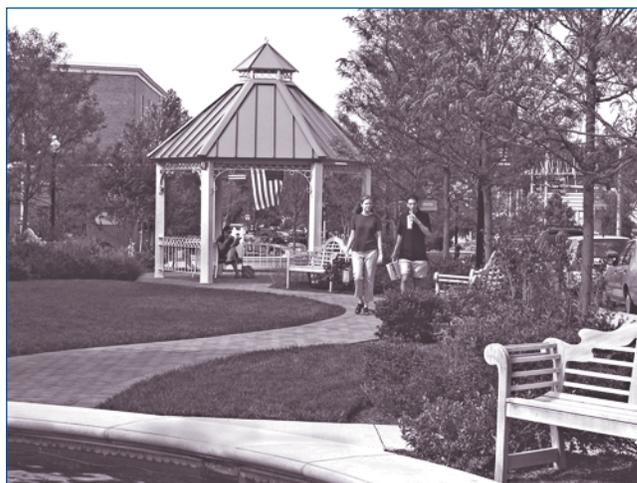
- Lighting Restrictions:** Flashing, traveling, animated, or intermittent lighting shall not be permitted.
- Lighting Plans:** Comprehensive lighting plans shall be provided for all new institutional, office, industrial, mixed-use and retail/commercial buildings.  
These lighting plans shall be accompanied by plans, sketches, or photographs indicating the design, size, methods of lighting fixture attachment, and other information required by the Permit Review section or Planning Board (for permit review and detailed site plan review, respectively).
- General Lighting Requirements:** Illumination shall be provided for main entrances, passageways, parking lots, dumpster, loading and recycling areas, service entrances and areas, alleys, pathways, open space, and plazas.  
Light fixtures for primary entrances shall provide light with a high color rendering index (a minimum of 65 CRI). Alleys used for service or parking access, parking areas, dumpsters, and service entrances shall be illuminated with pedestrian-oriented lighting located no greater than 16 feet above ground level.
- Residential Building Lighting:** Porch and/or entry lights shall be required on all residential or mixed-use buildings with residential components to create a safe pedestrian environment at night for residents.
- Light Shielding and Light Pollution:** All lighting shall be shielded with full cut-off light fixtures to minimize glare, sky-glow, and light pollution.



## D. Landscaping, Buffering, and Screening Standards

***Intent:** Attractive landscaping provides a wealth of benefits for a community, from a psychological sense of wellbeing to tangible benefits such as increased property values. Streets and lots utilizing trees, flowering plants, shrubs, and high-quality walls and fencing contribute to a positive identity, screen unattractive uses, parking areas, and mechanical equipment, provide shade, and create a pleasant and comfortable environment.*

1. **Public Spaces:** Public spaces shall be planted with shade and flowering trees, evergreen shrubs, and other appropriate landscaping to provide shade, increase air quality, and treat stormwater, as well as to add interest, visual appeal, and year-round greenery and color. Other devices, such as trellises, covered walkways, pavilions, and gazebos are also encouraged in public spaces to mark special locations and contribute to sense of place.



2. **Afforestation:** Afforestation shall be accomplished through the provision of shade and ornamental trees for all sites. Tree canopy coverage shall be provided for a minimum of 20 percent of the gross site area for sites that are exempt from the Woodland Conservation Ordinance.

Exceptions to this standard shall be considered on redevelopment sites where the provision of 10-year tree canopy coverage is not feasible due to existing buildings and site features.

The planting of trees on sites for new development and/or redevelopment shall be counted toward meeting the Woodland Conservation Ordinance requirements (if applicable). Street trees planted on abutting road rights-of-way may also be counted toward meeting that requirement.



3. **Bufferyards:** Bufferyard requirements are waived throughout the Old Town Bowie Development District.
4. **Service and Loading Areas:** Dumpsters and storage, service, loading, and delivery areas shall be hidden from public streets, walks, and from all adjacent property containing residential, commercial, and mixed-uses by utilizing landscaping, buffer walls, or other methods to screen the equipment.

5. **Materials:** Walls and fences shall be made of high-quality materials, such as brick, stone, wrought iron, and wood that are compatible with the associated building, or, if none, the adjacent buildings.

Chain-link fencing, barbed wire, unclad cinder block, corrugated metal, corrugated fiberglass, sheet metal, and wire mesh are not permitted.



## Parking Standards

***Intent:** The Parking Standards cover two areas: parking requirements and circulation and parking area design. For Old Town Bowie, the intent of the parking requirements section is to reduce the on-site parking requirements that are often difficult for small business and property owners to meet, while encouraging the provision and use of shared parking facilities to ensure that sufficient parking is available to support the businesses of Old Town. Shared parking areas and a reduced number of required parking spaces also reduce paved areas and provide increased opportunities for landscaping, buildings, and open space, contributing to the quality of the visual environment.*

### A. Parking Requirements

1. **Minimum Requirements:** The minimum number of off-street surface parking spaces permitted for each land use type shall be equal to 50 percent of the minimum number of required off-street parking spaces in accordance with Section 27-568(a) of the Zoning Ordinance.

Development of less than 1,000 square feet of total gross floor area (GFA) has no minimum parking requirements and is not subject to the requirements of Section 27-568(a) of the Zoning Ordinance.

Parking for residential and live/work uses shall consist of a minimum of one and a maximum of two off-street parking spaces per dwelling unit.
  2. **Maximum Requirements:** The maximum number of off-street surface parking spaces permitted for each land use type shall be equal to 80 percent of the minimum number of required off-street parking spaces in accordance with Section 27-568(a) of the Zoning Ordinance. If structured parking is provided or a Parking District is established, this maximum number may be increased.
  3. **Parking Location:** Parking may be located on- or off-site but shall be located within one-quarter mile of the development site. When off-site parking is used to meet the parking requirements, the applicant shall provide satisfactory documentation to show that parking is provided off-site.
  4. **Shared Parking:** To facilitate shared parking within the Development District, Section 27-570, Multiple Uses, and Section 27-272, Joint Use of a Parking Lot, shall be waived. The maximum parking requirements stated in 2. above are waived for shared parking areas (there is no maximum number of parking spaces for shared parking).
- a. **Single ownership:** For any property under one ownership and used for two or more uses, the number of parking spaces shall be computed by multiplying the minimum amount of parking required for each land use, as stated under number 1. above, by the appropriate percentage as shown in the shared parking requirements by time period (See 5, below). The number of parking spaces required for the development is then determined by adding the results for each column. The column totaling the highest number of parking spaces becomes the minimum off-street parking requirement.
  - b. **Multiple ownership:** The off-street parking requirements for two or more uses with different ownership may be satisfied by providing a joint parking facility, and the minimum parking requirements may be reduced in accordance with the procedure outlined above for shared parking for single ownership.
  - c. **Where shared parking is utilized,** the applicant shall provide details of the development's proposed uses and required parking along with a letter from the parking lot manager certifying that the lot has the capacity to accommodate all parking needs and that the parking owner has entered into an agreement to share the number of spaces required.

**5. Shared Parking Percentage Requirements**

Type of Use	Weekday		Weekend		Nighttime
	Daytime 6:00 a.m.– 6:00 p.m.	Evening 6:00 p.m.– Midnight	Daytime 6:00 a.m.– 6:00 p.m.	Evening 6:00 p.m.– Midnight	Midnight– 6:00 a.m.
Office/Industrial/ Manufacturing/Storage	100	10	10	5	5
Commercial Trade	60	90	100	70	5
Restaurant	50	100	100	100	10
Lodging	70	100	70	100	70
Recreational/Entertainment Social/Cultural	40	100	80	100	10
Residential	60	90	80	90	100
Other Uses	100	100	100	100	100

Source: Table based on “Shared Parking,” a publication of the Urban Land Institute, Washington, D.C., 1983.

6. **Parking District:** To promote economic development and facilitate infill and redevelopment in Old Town Bowie, the Planning Board during the site plan review process (for detailed site plan review only) may reduce or waive the minimum off-street parking requirements provided that:

- a. A Parking District is established for the area in accordance with the requirements of Subtitle 2, Division 27, Sections 2-399 to 2-413 of the Prince George’s County Code.
- b. The applicant agrees to pay a fee-in-lieu for the required number of off-street parking spaces that are to be waived and/or a special assessment as defined by the Parking District.

7. **On-Street Parking:** Each 20 feet of linear frontage on a street with on-street parking may be substituted for one space to satisfy the minimum off-street parking requirements.

## B. Parking and Loading Area Design

*Intent: Parking lots are not a primary use but are an accessory use. As such, they should not dominate the streetscape, obscure building frontages, endanger pedestrians, or overwhelm the visual environment. Ensure that parking lots in Old Town Bowie are designed and located so that they are attractive, do not inhibit vehicular or pedestrian movement and safety, are appropriately screened from view from public streets and open space, and reduce curb cuts to minimize potential pedestrian/vehicular conflicts. Planting and landscaping areas should be large enough to allow for healthy tree growth and located where trees will be protected from car overhangs and opening car doors.*

1. **Parking Structure Location:** Structured parking shall be located on the interior of the block or at the rear of the property, and shall be accessed from a side street, alley, or entrance drive-aisle.

Residential unit garages shall be located at the rear of the property and be accessed from a side street, alley or landscaped access driveway. Residential garages shall be recessed a minimum of 6 feet from the front building façade of the residential structure.



2. **Parking Lot Landscaping:** Parking lots shall be screened from roadways and public areas (such as sidewalks, plazas, and abutting open space) with appropriate landscaping, a continuous low (3 to 4 feet in height) masonry wall, or other appropriate screening techniques. Landscaping shall be provided in surface parking lots, as specified in the following requirements.

These requirements replace the requirements of Section 4.3, Parking Lot Requirements, of the Landscape Manual.

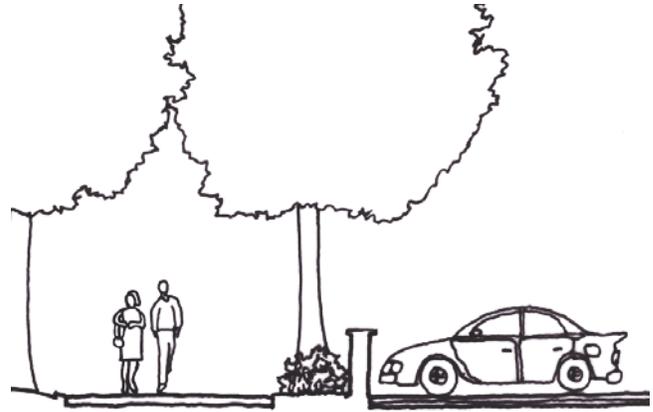


3. **Landscaped Strip:** A landscaped strip consisting of a minimum 4-foot-wide landscaped strip between the right-of-way line and the parking lot, with a masonry wall or thick shrubbery wall between 36 and 48 inches in height, shall be provided to screen the parking lot. The wall shall be located adjacent to but entirely outside the 4-foot-wide landscaped strip. Plant with a minimum of 1 shade tree per 35 linear feet of frontage, excluding driveway opening, and with a mixture of evergreen groundcover and low shrubs planted between the shade trees.



4. **Perimeter Landscaping:** Perimeter landscaping from incompatible uses as defined in Section 4.7 of the Landscape Manual (where this may apply within the Old Town Bowie DDOZ) shall consist of:

- a. A landscaped strip at least 10 feet wide between the right-of-way and the parking lot, to be planted with a minimum of 1 shade tree and 10 shrubs per 35 linear feet of frontage, excluding driveway openings.
- b. A landscaped strip to be a minimum of 4 feet wide, with a minimum 3-foot-high masonry wall, and/or plantings to consist of at least 1 tree and 3 shrubs per 35 linear feet of parking lot perimeter adjacent to a property line.
- c. A combination of the above.



If walls are constructed, they shall be located adjacent to but entirely outside the 4-foot-wide landscaped strip and shall provide at least one passage with a minimum of 3 feet in width per every 60 linear feet when the wall is adjacent to open space, a pedestrian path, public plaza, or other pedestrian-oriented space to facilitate pedestrian movement and foster connections between parking areas and nearby uses.

5. **Interior Planting:** Interior planting shall be required for any parking lot which is 6,000 square feet or larger. At least six percent of the lot must be interior planting area.

For purposes of calculation of the required area, all areas within the perimeter of the parking lot shall be counted, including planting islands, curbed areas, corner areas, parking spaces, and all interior driveways and aisles except those with no parking spaces located on either side.

Landscaped areas situated outside the parking lot, such as peripheral areas and areas surrounding buildings, may not be counted as interior planting area. In order to count toward meeting the requirement, landscaping must provide shade to the paved surfaces (to be indicated on submitted site plans as necessary).

If a parking lot less than 6,000 square feet is built without interior landscaping and later, additional spaces are added so that the total size of the lot is greater than 6,000 square feet, then interior landscaping shall be provided for the entire lot.

6. **Interior Planting Requirements:** At least 1 shade tree shall be provided for each 300 square feet (or fraction) of interior landscaped area provided. These trees shall have a clear trunk at least six feet above finished grade level.

A minimum of 60 square feet of continuous pervious land area shall be provided for each tree. No tree planting area shall be less than 5 feet wide in any dimension.

A curb or wheelstop shall be provided for all parking spaces adjacent to planting or pedestrian areas to protect those areas from overhanging by parked vehicles.

Planting islands located parallel to parking spaces shall be a minimum of 9 feet wide to allow car doors to swing open.

In cases where a planting island is perpendicular to parking spaces and the spaces head into the planting island on both sides, the island shall be a minimum of 8 feet wide to allow for bumper overhang. If parking spaces are located on only one side of such a planting island, the island shall be a minimum of 6 feet wide.

7. **Planting Substitutions:** Where the plantings required above would result in an inappropriate or impractical design due to underground utilities, overhead wires, or other factors, the following will apply:
  - a. Two ornamental trees may be substituted for one shade tree.
  - b. Two evergreen trees may be substituted for one shade tree.
  - c. One evergreen tree may be substituted for five shrubs.

However, evergreen trees shall not be used as street trees. If a substitution is required for a street tree, ornamental trees shall be used instead. Consult the Landscape Manual for recommended species.

8. **Materials:** Parking, loading, and service area screening walls and fences shall be made of high-quality materials such as brick, stone, finished decorative concrete, wrought iron, and wood.

Chain-link fencing, barbed wire, unclad cinder block, corrugated metal, corrugated fiberglass, sheet metal, and wire mesh are not permitted.

9. **Screening for Loading and Service Areas:** Loading and service areas shall not be visible from streets. Loading and service areas shall be screened with landscape plantings and/or a 6-foot high opaque wood fence or masonry wall and shall be located a minimum of 30 feet away from public sidewalks.

10. **Drive-in and Drive-Through Windows:** Drive-in or drive-through windows for any new use, other than for banks, shall not be permitted. Drive-in or drive-through windows for banks shall be located to the rear of the lot and shall not front the street under any circumstances.

## C. Bicycle Parking

*Intent: Regulate the design and location of bicycle parking facilities to provide convenient access to businesses and amenities in Old Town Bowie. Provide sufficient spaces at appropriate locations to encourage bicycle use and reduce dependency on the automobile as a means of transportation. Enhance the quality of the streetscape environment through the provision of attractive and durable bicycle racks.*

1. **Bicycle Space Provisions:** The minimum number of required bicycle parking spaces shall be:
  - One space per every two dwelling units (multifamily residential uses)
  - One space per every five dwelling units (senior housing/residential marketed uses)
  - Two spaces per 5,000 square feet of park, plaza, and open space
  - Two spaces per 6,000 square feet of office and commercial uses, with four minimum
  - Two spaces per 4,000 square feet of recreational, entertainment, social or cultural uses, with six minimum

Single-family dwellings and live/work units are exempt from this requirement.

2. **Bicycle Space Dimensions:** Bicycle spaces shall be a minimum of 6 feet long and 2.5 feet wide, and shall provide an overhead minimum clearance of 7 feet in covered spaces. A minimum 5-foot-wide clear aisle shall be provided between each row of bicycle parking.
3. **Bicycle Parking Locations:** Bicycle parking areas shall be convenient to the entrances of all businesses, multifamily dwellings, and public and quasi-public buildings. Bicycle parking areas shall be located within 50 feet of the main entrance, and have direct access to the public right-of-way.

Bicycle parking may be located within a building or parking structure, but shall remain near entrances, be easily accessible, and incorporate safety measures such as visually transparent walls and high color index lighting.

Bicycle parking areas shall not obstruct sidewalks or walkways.



4. **Bicycle Furniture:** At least one U-shaped rack, bicycle locker, or space on a multiple-space bicycle rack shall be provided for each required bicycle parking space. Bicycle racks, lockers, and other bicycle amenities shall be securely anchored to the ground.

